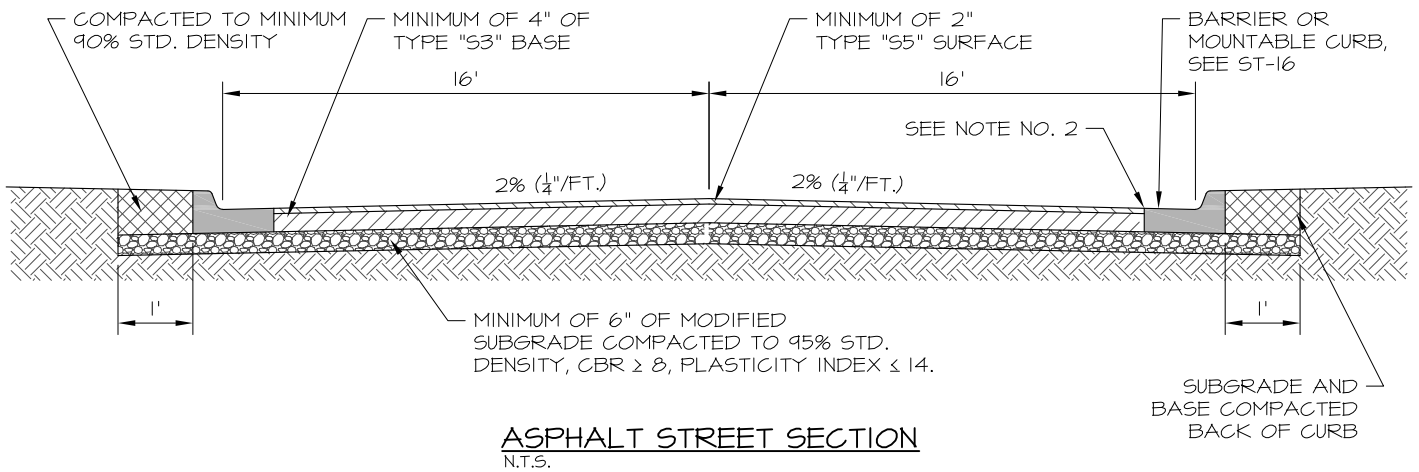
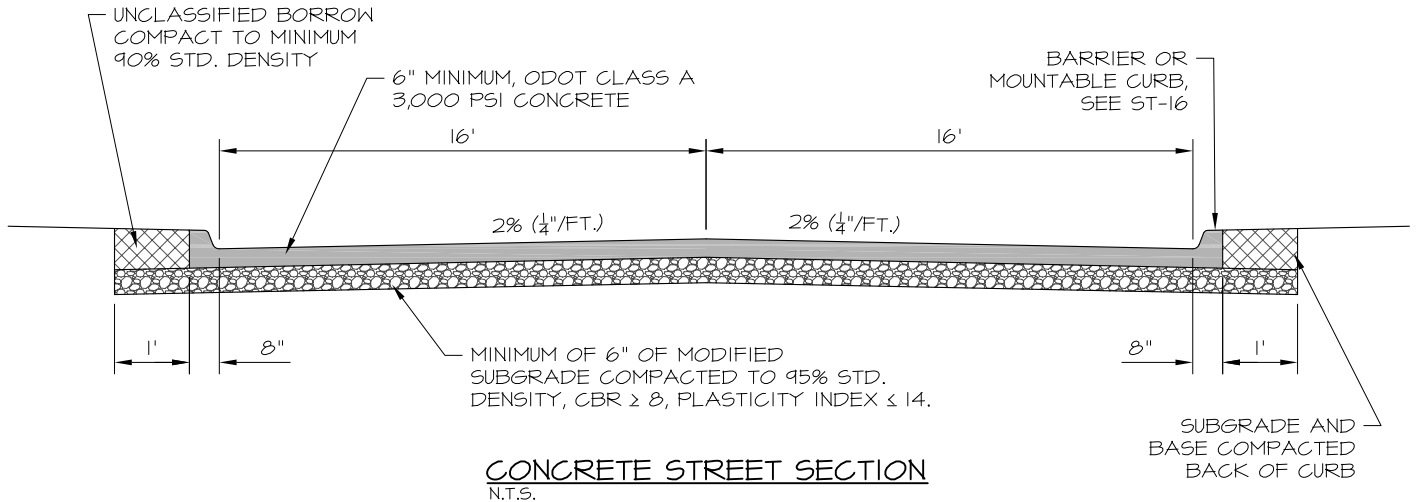
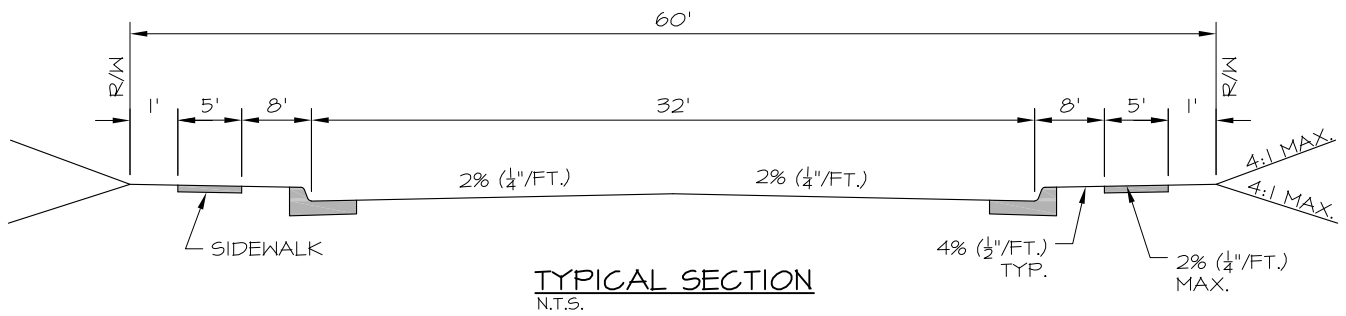


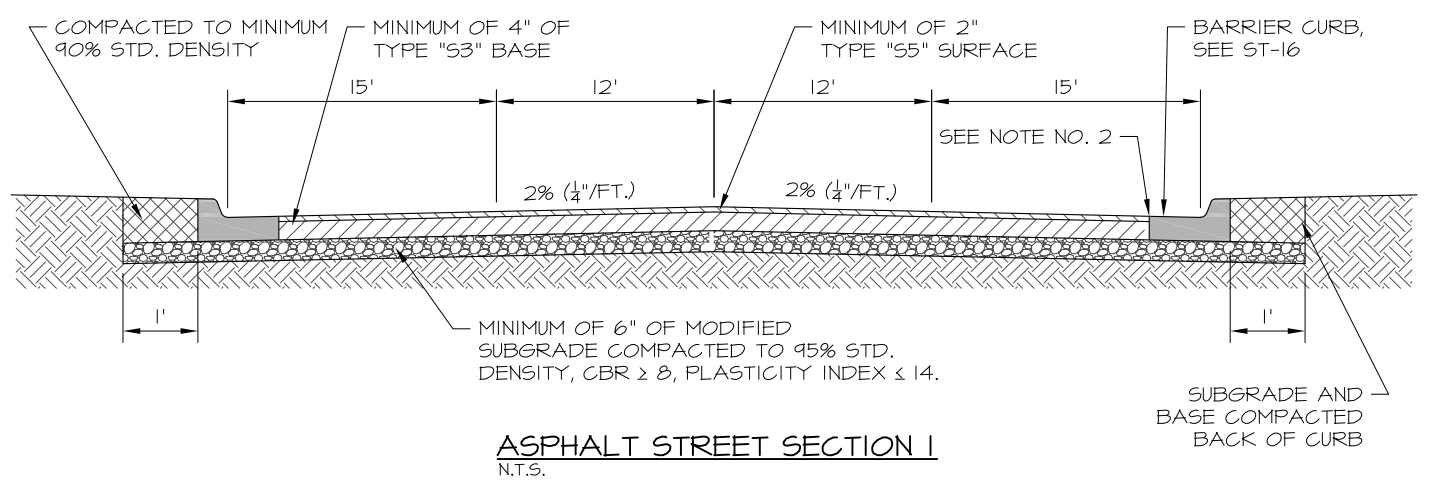
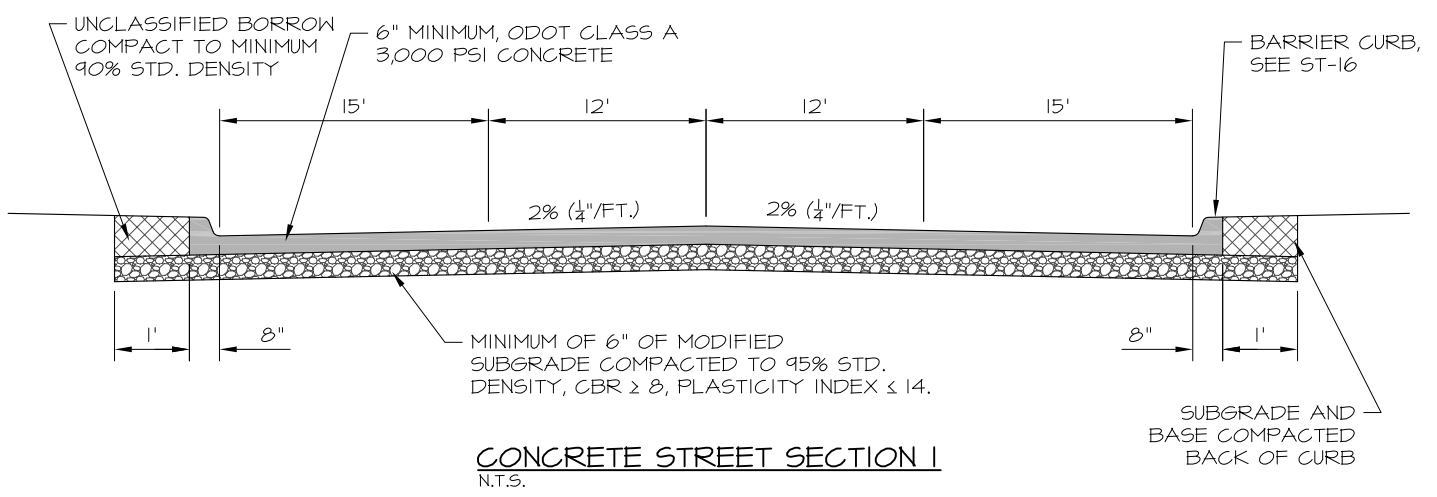
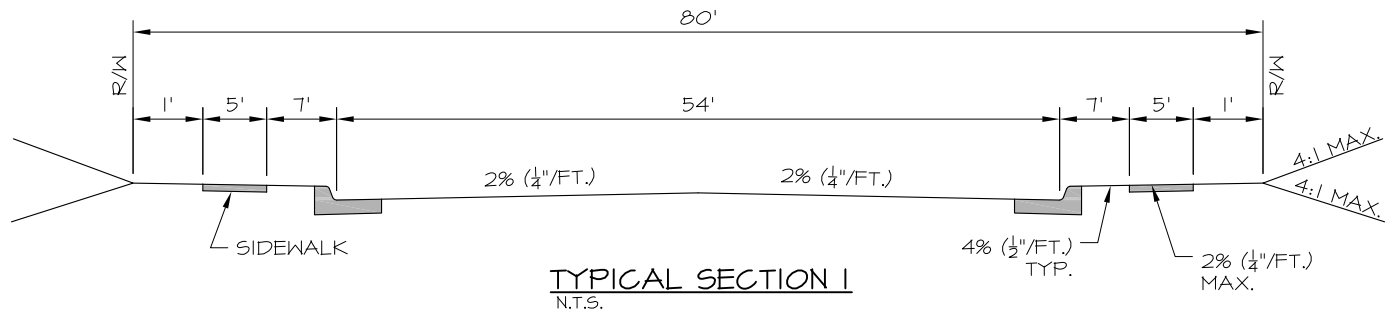
NOTES:

1. PAVING SECTION SHOWN IS MINIMUM ALLOWED, STREET PAVING SHALL BE DESIGNED IN ACCORDANCE WITH THE GEOTECHNICAL RECOMMENDATIONS.
2. ASPHALT SURFACE SHALL BE 1/4" ABOVE EDGE OF CONCRETE GUTTER. THE GUTTER MAY BE REDUCED TO 5/8" TO ACCOMMODATE THIS REQUIREMENT.



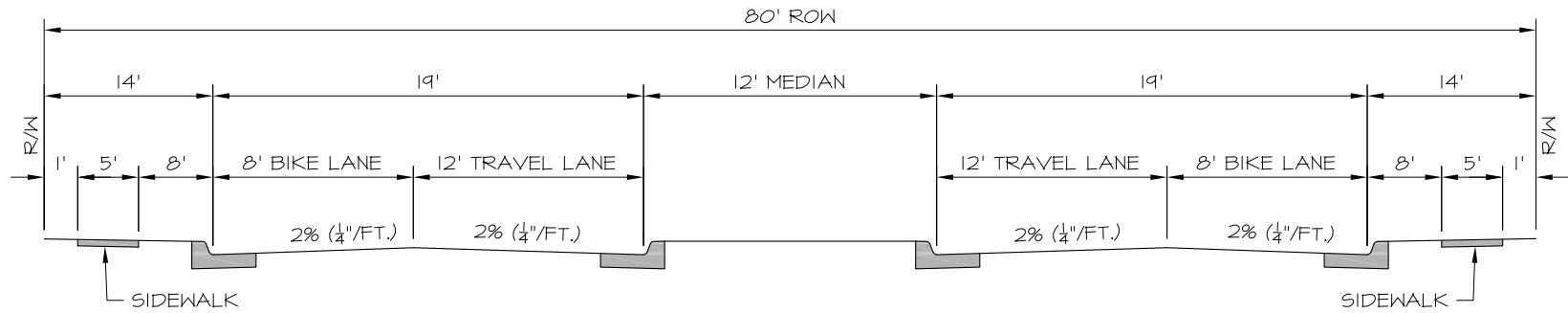
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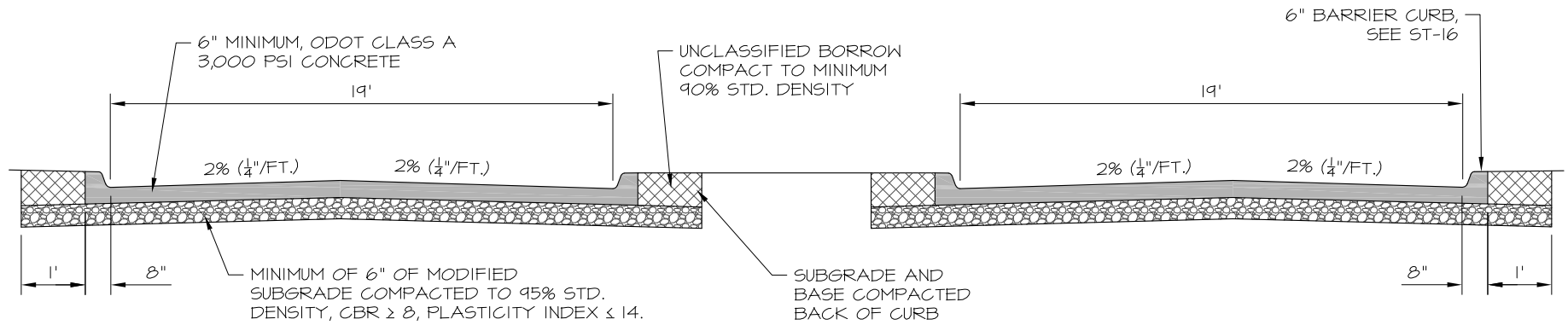


NOTES:

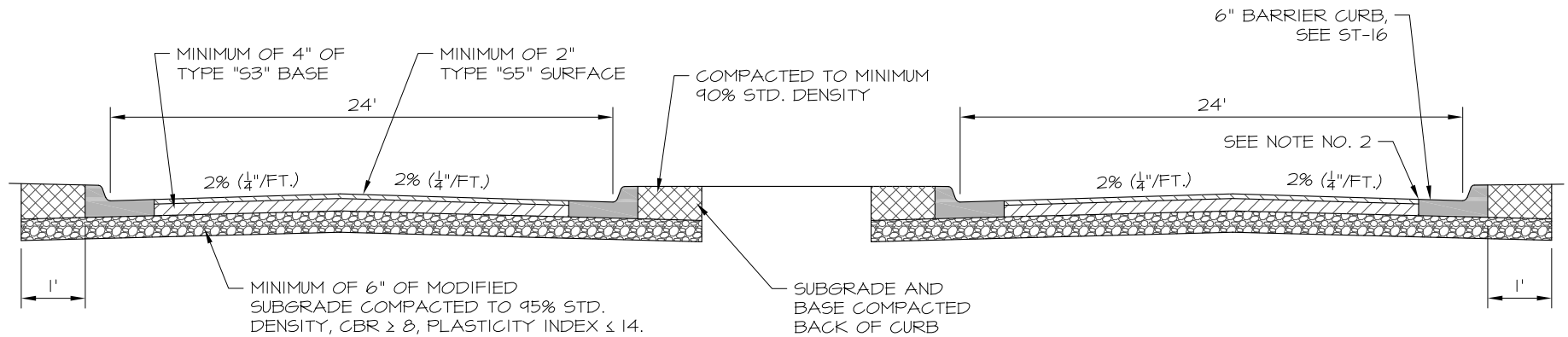
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TYPICAL SECTION II
N.T.S.



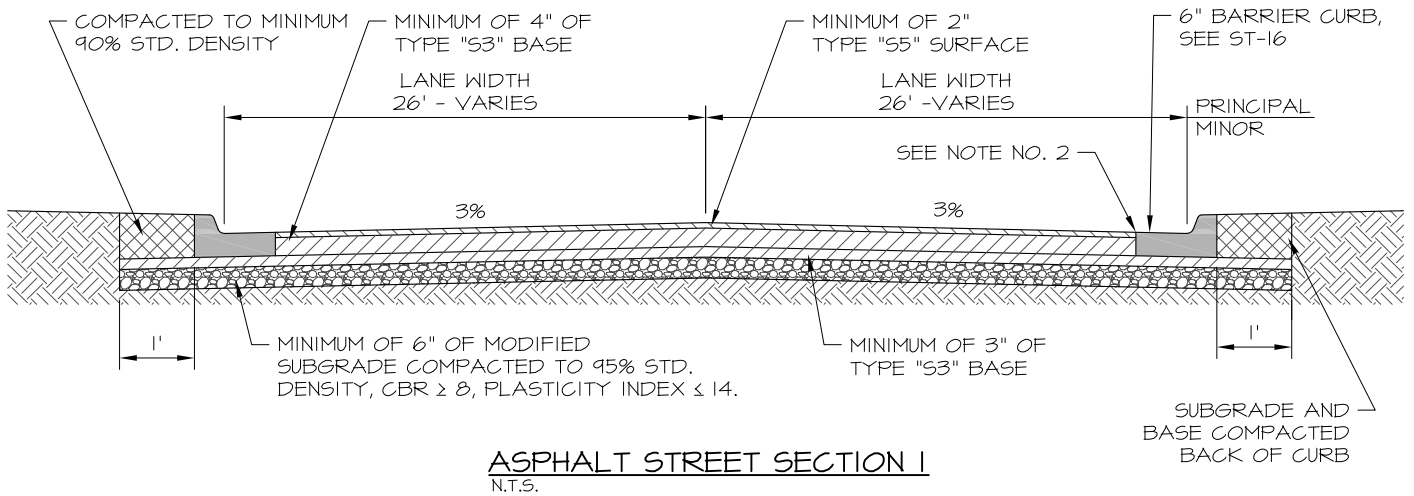
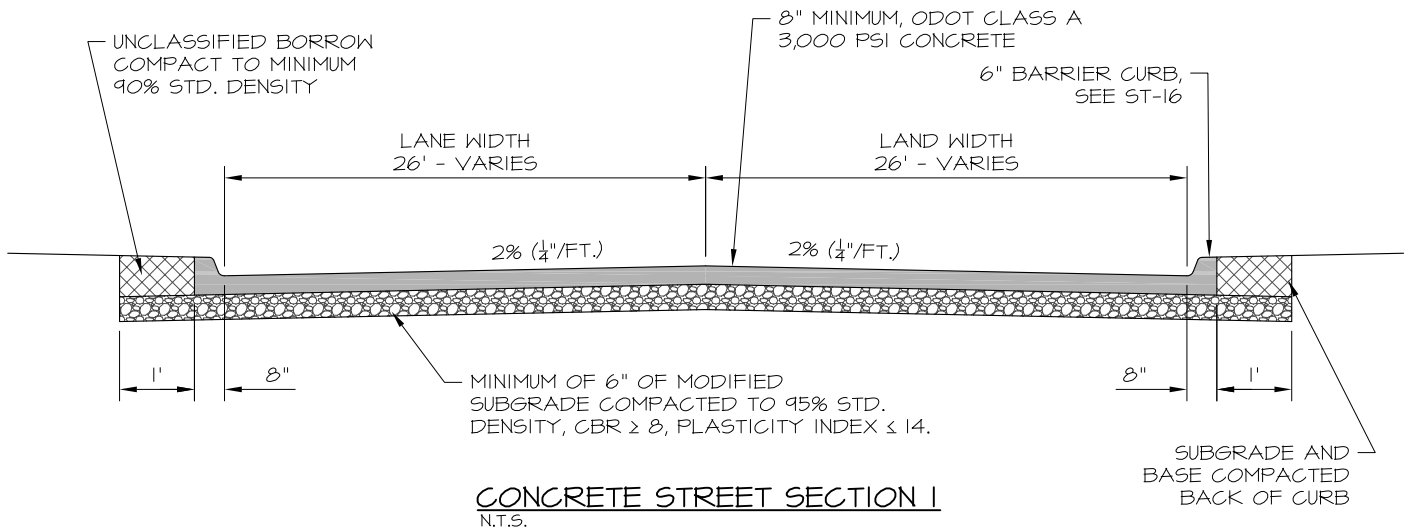
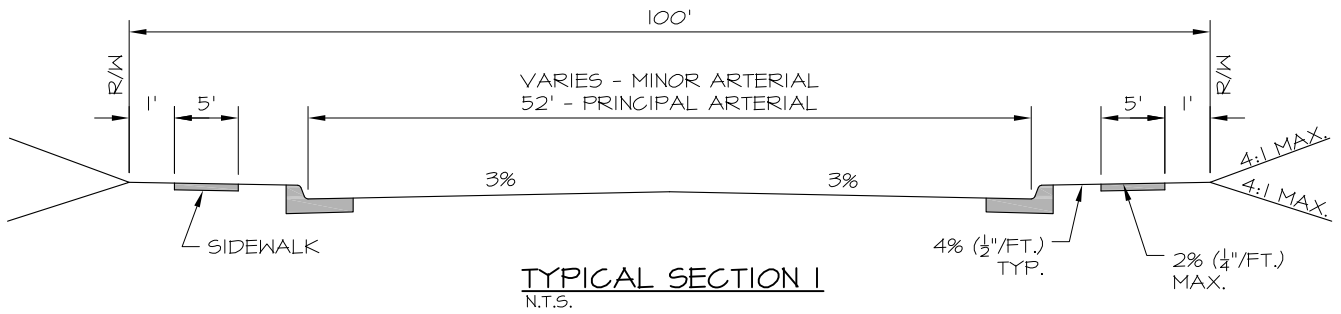
CONCRETE STREET SECTION II
N.T.S.



ASPHALT STREET SECTION II
N.T.S.

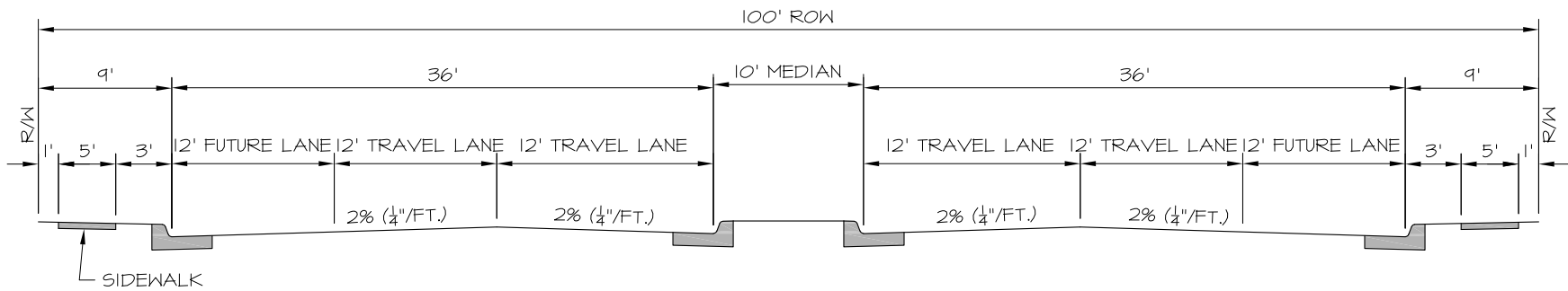
NOTES:

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2. ASPHALT SURFACE SHALL BE 1/4" ABOVE EDGE OF CONCRETE GUTTER. THE GUTTER MAY BE REDUCED TO 5 3/4 TO ACCOMMODATE THIS REQUIREMENT.

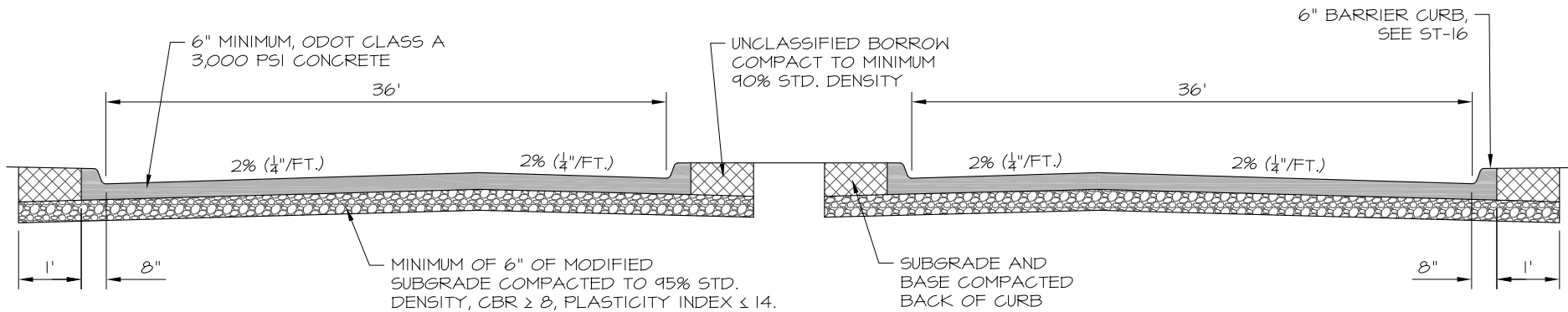


NOTES:

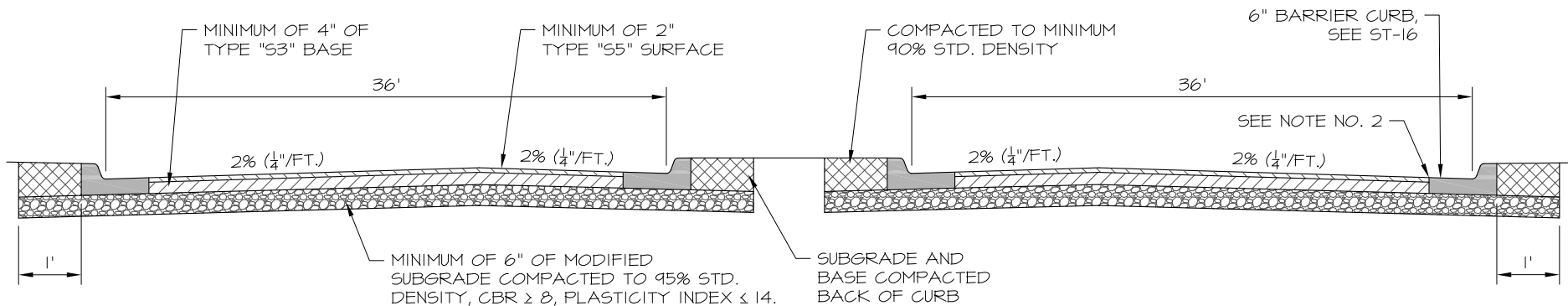
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2. ASPHALT SURFACE SHALL BE 1/4" ABOVE EDGE OF CONCRETE GUTTER. THE GUTTER MAY BE REDUCED TO 5/8" TO ACCOMMODATE THIS REQUIREMENT.



TYPICAL SECTION II
N.T.S.



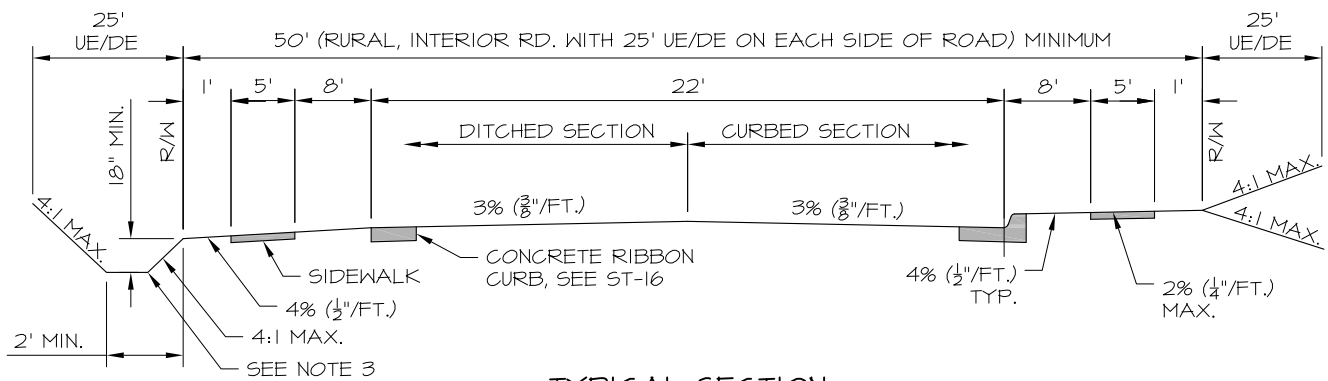
CONCRETE STREET SECTION II
N.T.S.



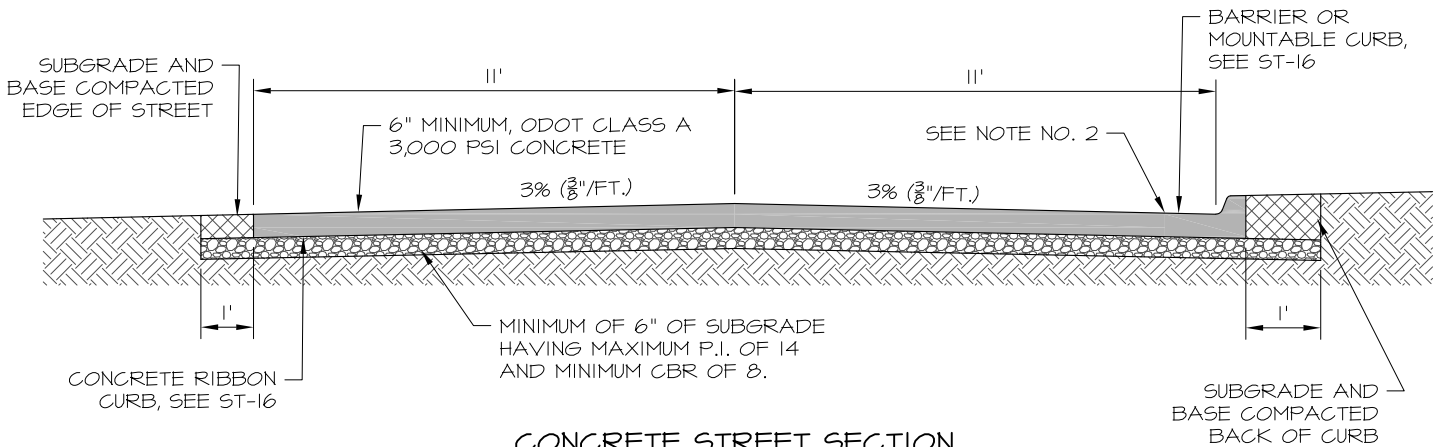
ASPHALT STREET SECTION II
N.T.S.

NOTES:

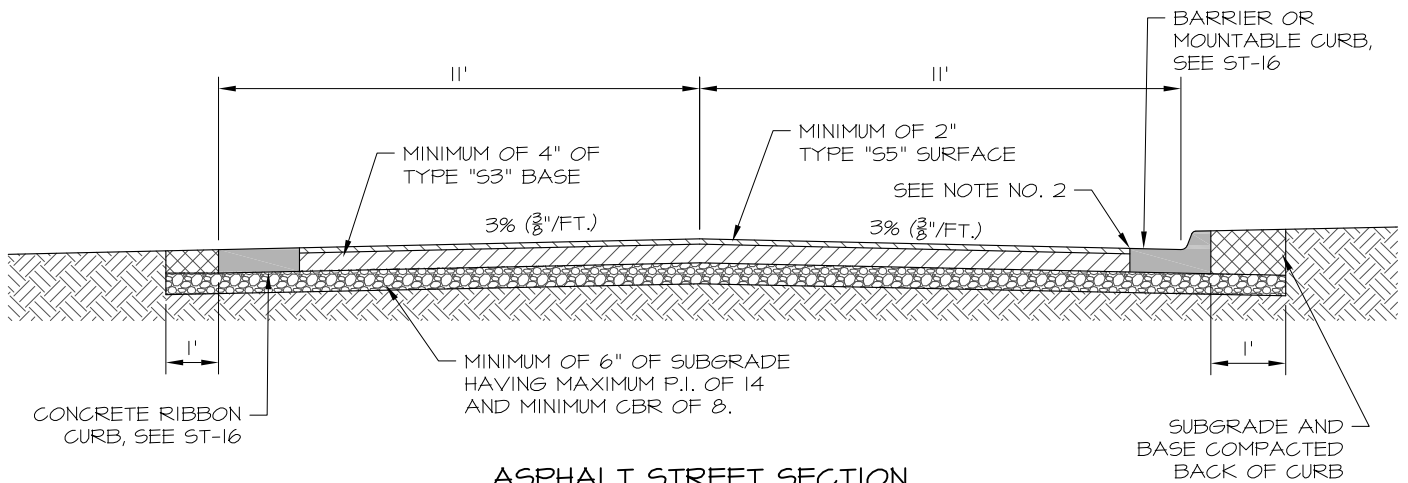
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2. ASPHALT SURFACE SHALL BE 1/4" ABOVE EDGE OF CONCRETE GUTTER. THE GUTTER MAY BE REDUCED TO 5 3/4" TO ACCOMMODATE THIS REQUIREMENT.



TYPICAL SECTION
N.T.S.



CONCRETE STREET SECTION
N.T.S.

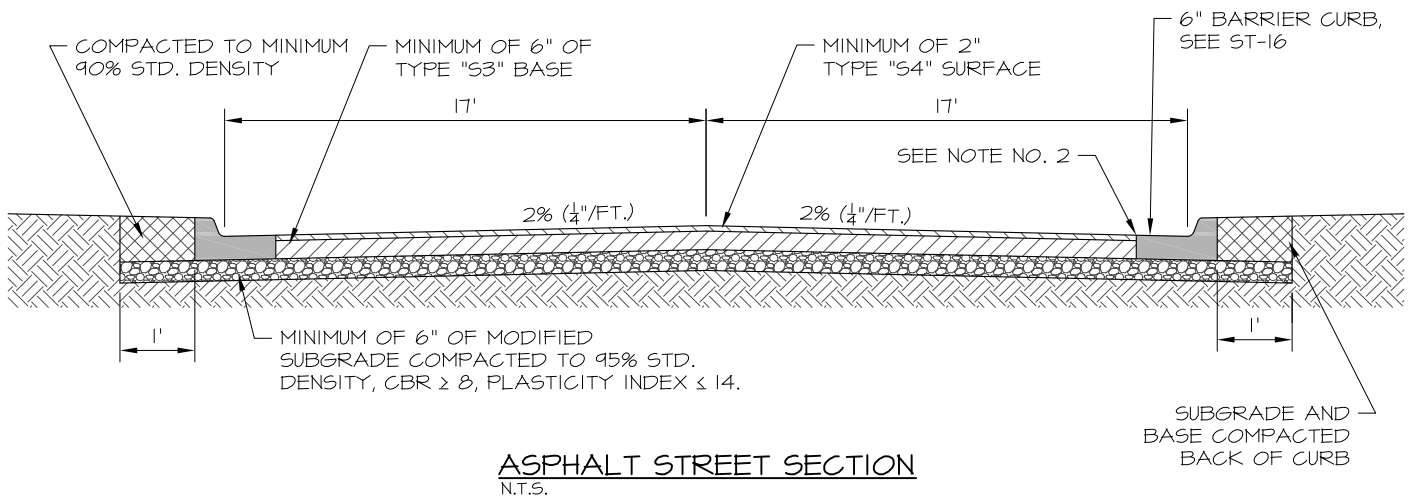
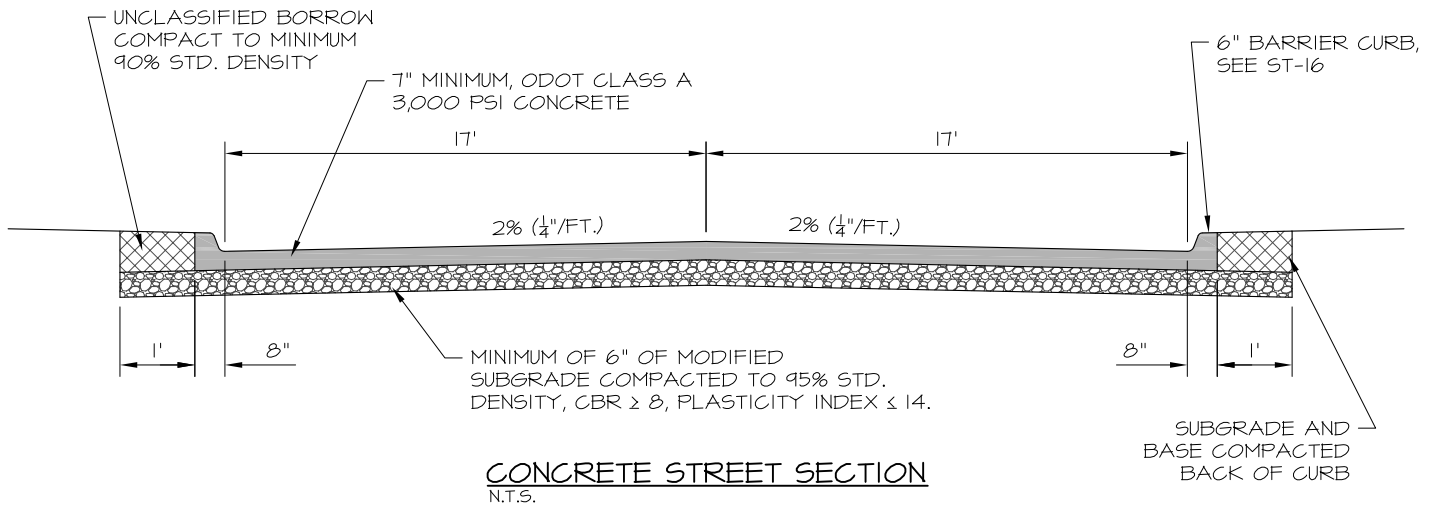
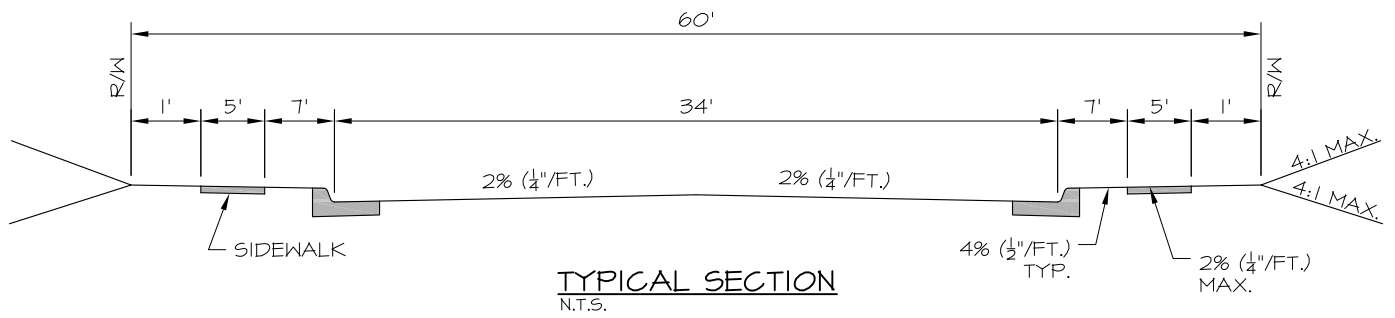


ASPHALT STREET SECTION
N.T.S.

NOTES:

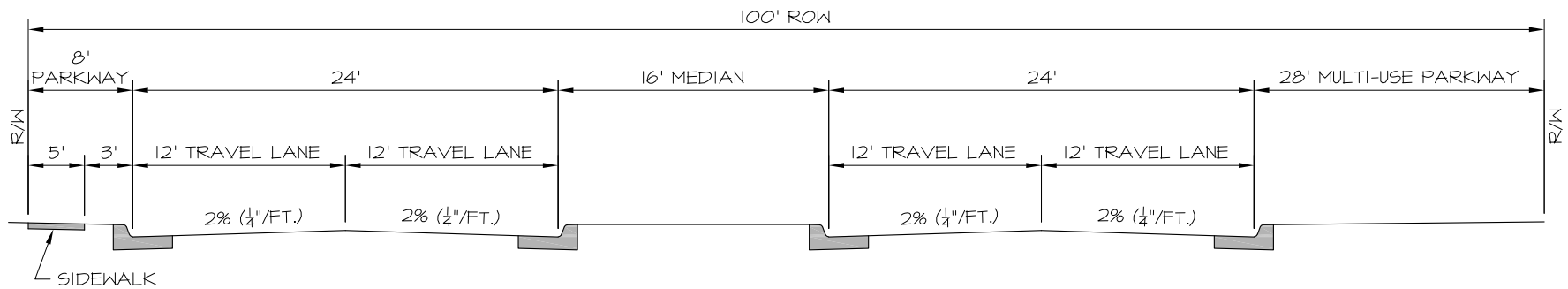
1. PAVING SECTION SHOWN IS MINIMUM ALLOWED, STREET PAVING SHALL BE DESIGNED IN ACCORDANCE WITH THE GEOTECHNICAL RECOMMENDATIONS.
2. ASPHALT SURFACE SHALL BE 1/4" ABOVE EDGE OF CONCRETE GUTTER. THE GUTTER MAY BE REDUCED TO 3/4" TO ACCOMMODATE THIS REQUIREMENT.
3. DITCH SECTION MAY ONLY BE USED WHEN LOT SIZE IS 1/2 ACRE OR LARGER AS FOLLOWS:
 - a.) MINIMUM EARTHEN DITCH GRADE SHALL BE 1%. MINIMUM CONCRETE LINED DITCH GRADE SHALL BE 0.5%
 - b.) WHERE DITCH CROSSES DRIVES THERE SHALL BE CONCRETE/HDPE PIPE WITH PRECAST, POURED IN PLACE OR MASONRY HEADWALLS (SEE ST-15). DITCH, HEADWALL, AND PIPES SHALL BE MAINTAINED BY THE PROPERTY OWNER.
 - c.) ALL DITCHES CROSSING STREETS SHALL HAVE HEADWALLS AND SHALL BE STAMPED AND STAINED CONCRETE. PUBLIC ROADS SHALL BE MAINTAINED BY THE CITY.
 - d.) ALL DITCHES SHALL BE OWNER MAINTAINED.

DESIGN STANDARD DETAILS
CITY OF CHOCTAW, OKLAHOMA
LOCAL (RUAL) PUBLIC / PRIVATE STREET

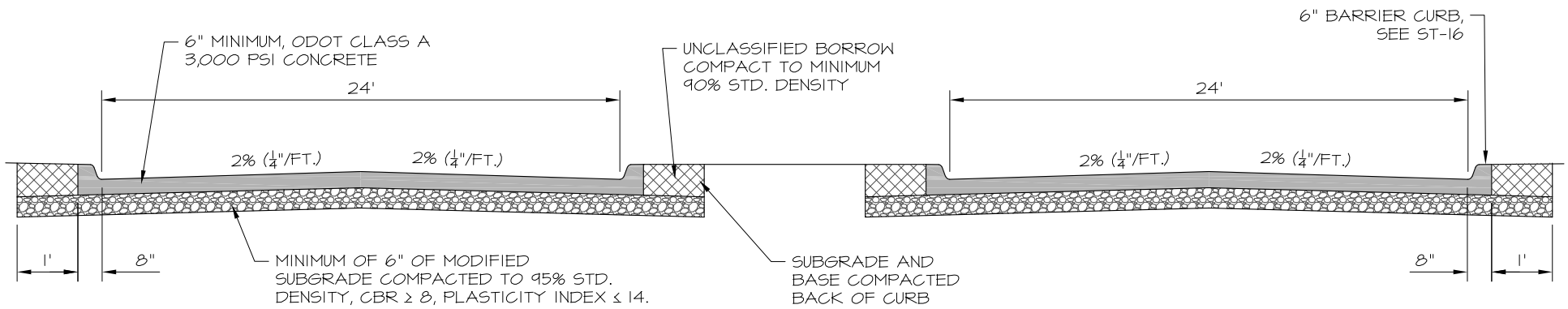


NOTES:

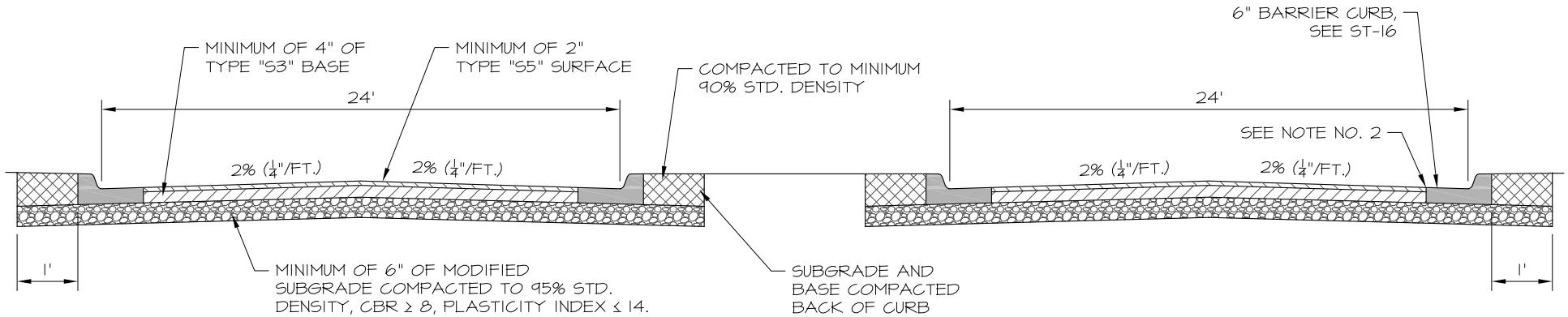
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2. ASPHALT SURFACE SHALL BE 1/4" ABOVE EDGE OF CONCRETE GUTTER. THE GUTTER MAY BE REDUCED TO 5 3/4 TO ACCOMMODATE THIS REQUIREMENT.
3. CURB THICKNESS TO BE 8" THICK, OR 2" THICK TYPE "S3" ASPHALT LAYER EXTENDED UNDER CURB SECTION.



TYPICAL SECTION
N.T.S.



CONCRETE STREET SECTION
N.T.S.



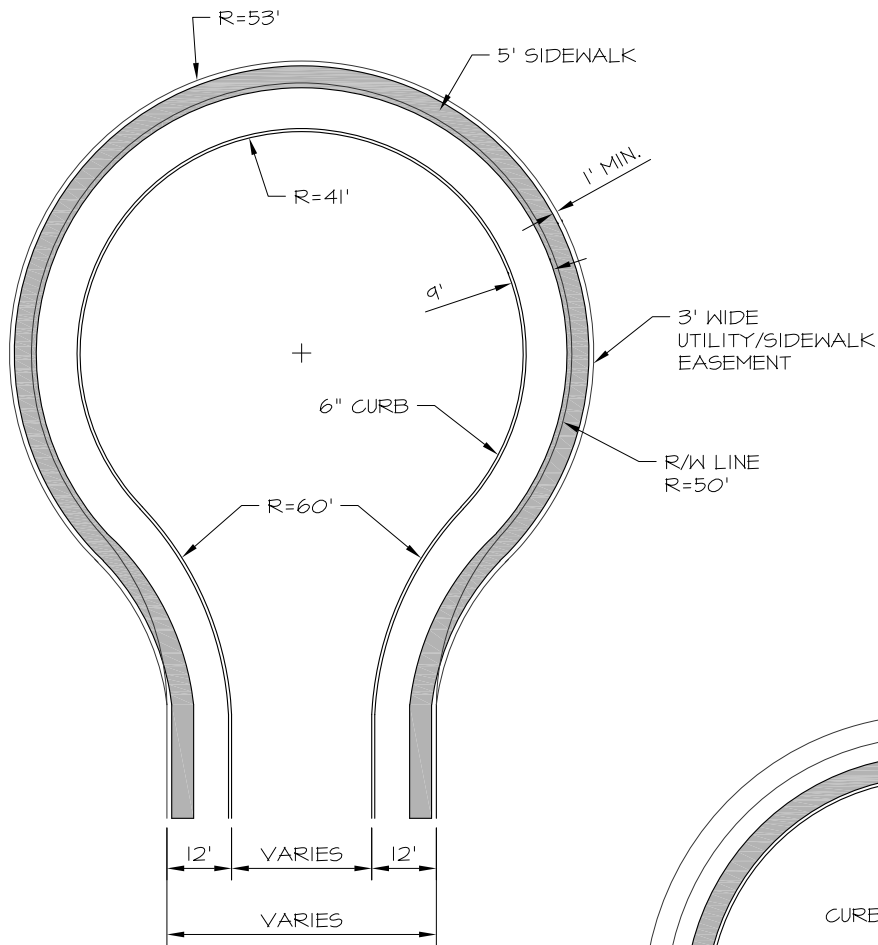
ASPHALT STREET SECTION
N.T.S.

NOTES:

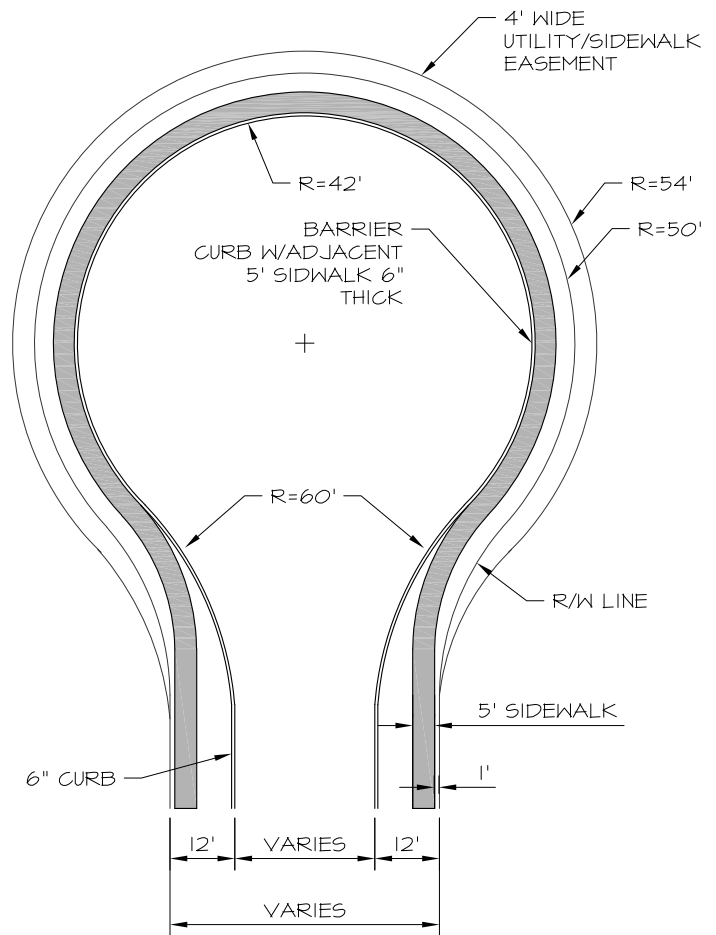
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2. ASPHALT SURFACE SHALL BE 1/4" ABOVE EDGE OF CONCRETE GUTTER. THE GUTTER MAY BE REDUCED TO 5 3/4" TO ACCOMMODATE THIS REQUIREMENT.

ST 09

DESIGN STANDARD DETAILS
CITY OF CHOCTAW, OKLAHOMA
PARKWAY CROSS-SECTION (100' ROW)



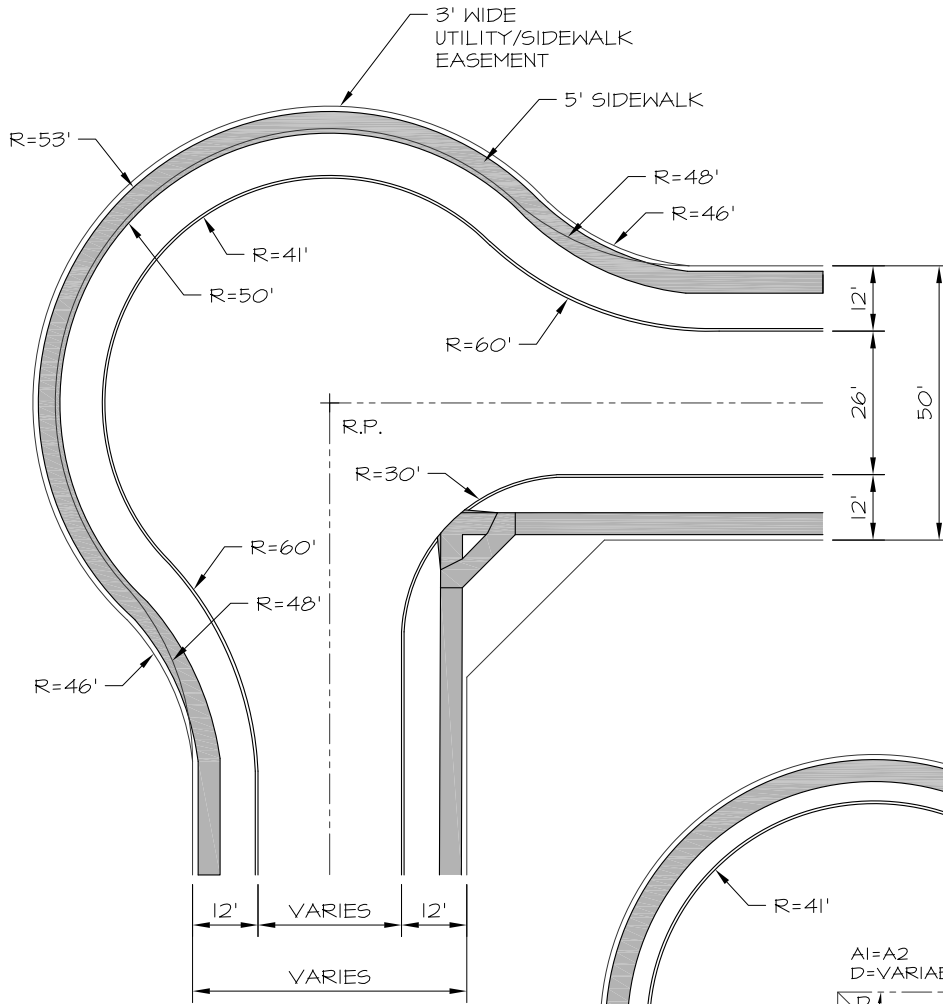
**UN-CURBED
CUL-DE-SAC**
N.T.S.



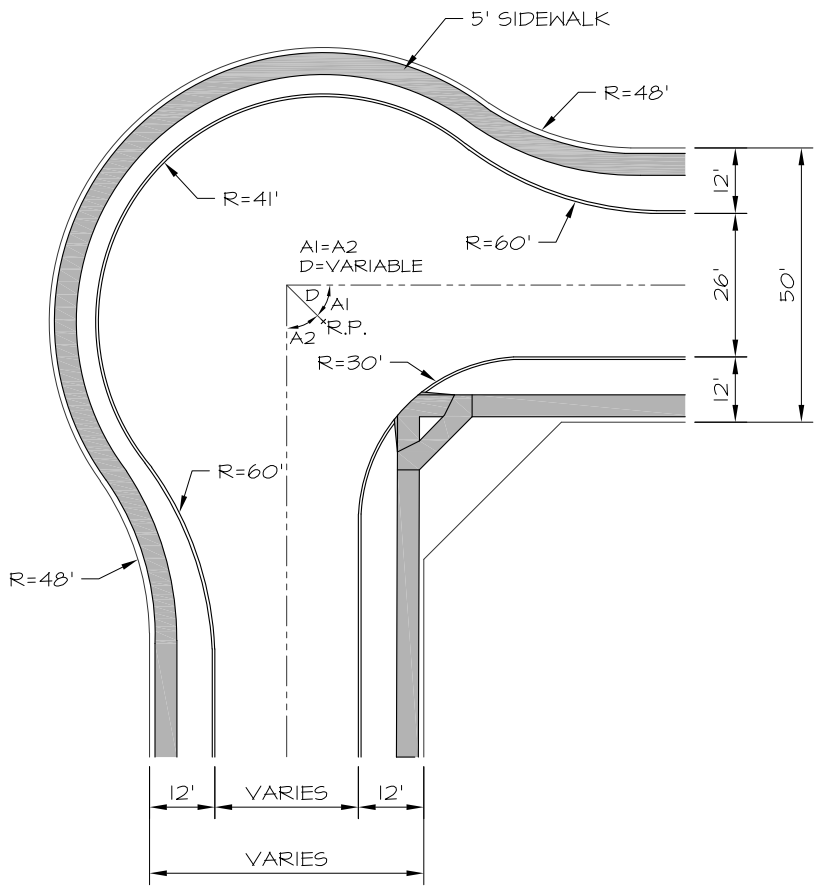
**CURBED
CUL-DE-SAC**
N.T.S.

NOTES:

1. WARNING SIGNS SHALL BE POSTED AT ENTRANCE TO STREET, INDICATING NO OUTLET.
2. DETAIL SHALL BE IDENTIFIED BY STREET NAME.
3. ELEVATIONS PROVIDED SHALL BE PROPOSED FLOW LINE OF GUTTER.
4. ONE DETAIL SHALL BE PROVIDED FOR EACH CUL-DE-SAC.
5. SCALE SHALL BE 1:500 (1"=40') OR LARGER.
6. MAILBOX NOT ALLOWED BEHIND SIDEWALK. CLUSTER BOXES REQUIRED FOR SIDEWALK ADJACENT TO CURB.



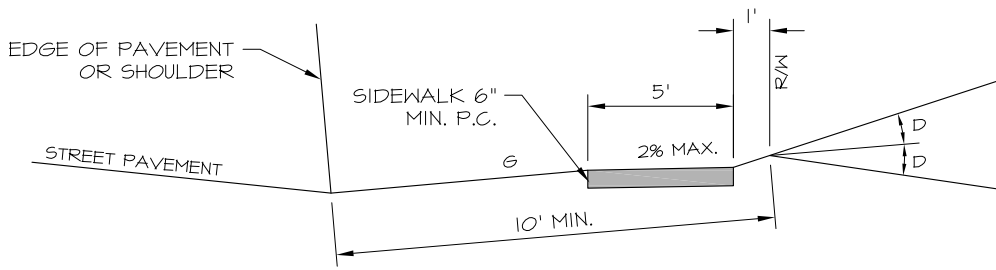
ELBOW
N.T.S.



ELBOW WITH
R.P. OFFSET
N.T.S.

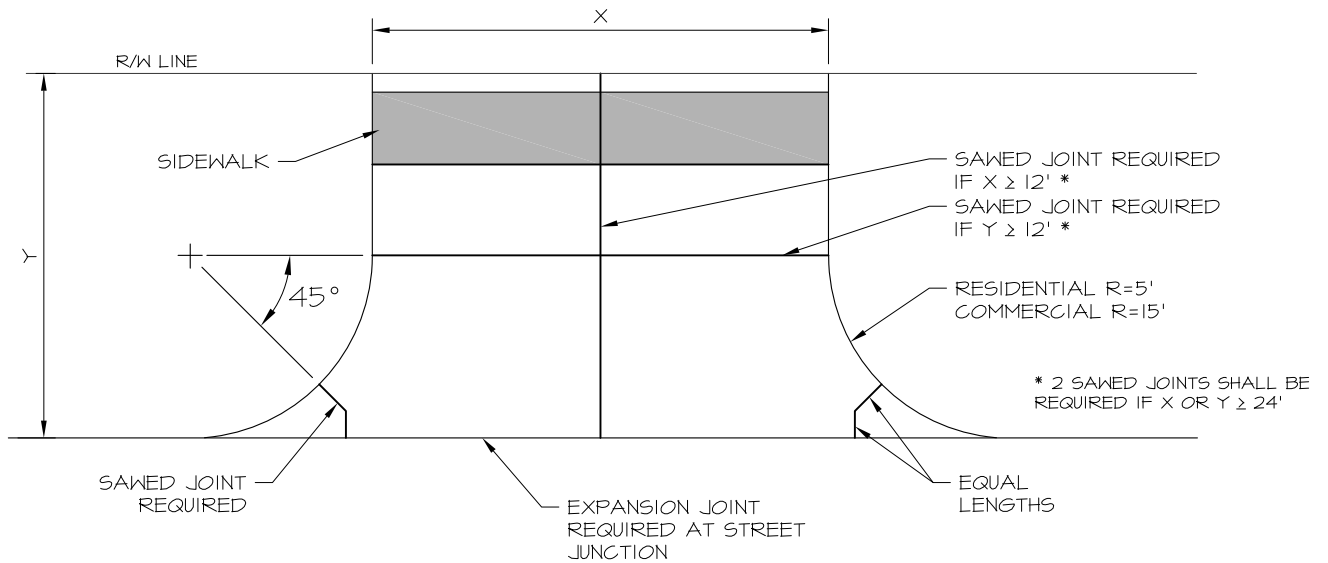
NOTES:

1. DISTANCE "D" : 10 FOOT MAXIMUM.
2. DETAIL SHALL BE IDENTIFIED BY STREET NAME.
3. ELEVATIONS PROVIDED SHALL BE PROPOSED FLOW LINE OF GUTTER.
4. ONE DETAIL SHALL BE PROVIDED FOR EACH RESIDENTIAL ELBOW.
5. SCALE SHALL BE 1:500 (1"=40') OR LARGER.
6. MAILBOX NOT ALLOWED BEHIND SIDEWALK. CLUSTER BOXES REQUIRED FOR SIDEWALK ADJACENT TO CURB.



CONDITION	MAX. (G)	MAXIMUM GRADE CHANGE (D)	
		DESIRABLE	MAXIMUM
HIGH VOLUME DRIVEWAY	6%	0%	+ 3%
LOW VOLUME DRIVEWAY ON ARTERIAL	6%	+ 3%	+ 6%
LOW VOLUME DRIVEWAY ON COLLECTOR	8% - 10%	+ 6%	+ 12%
LOW VOLUME DRIVEWAY ON LOCAL STREET	10%	+ 6%	+ 12%

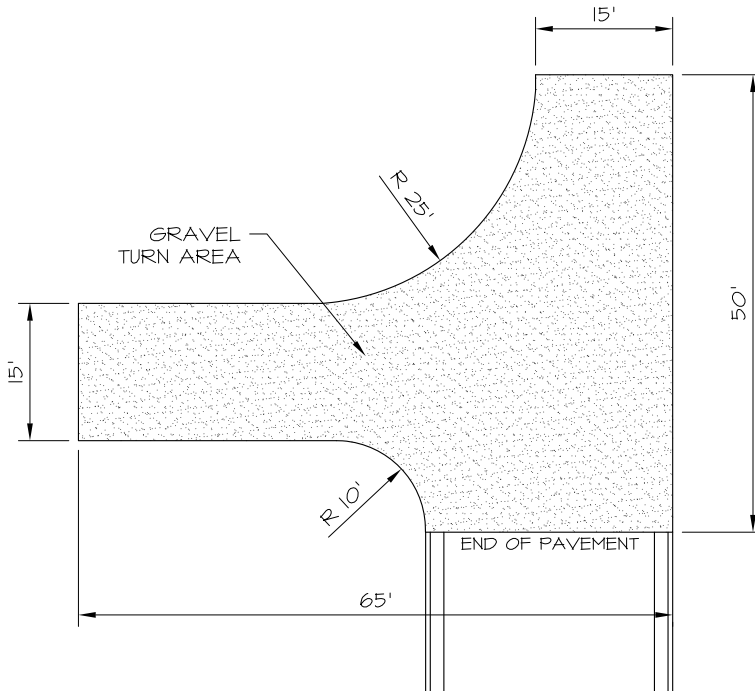
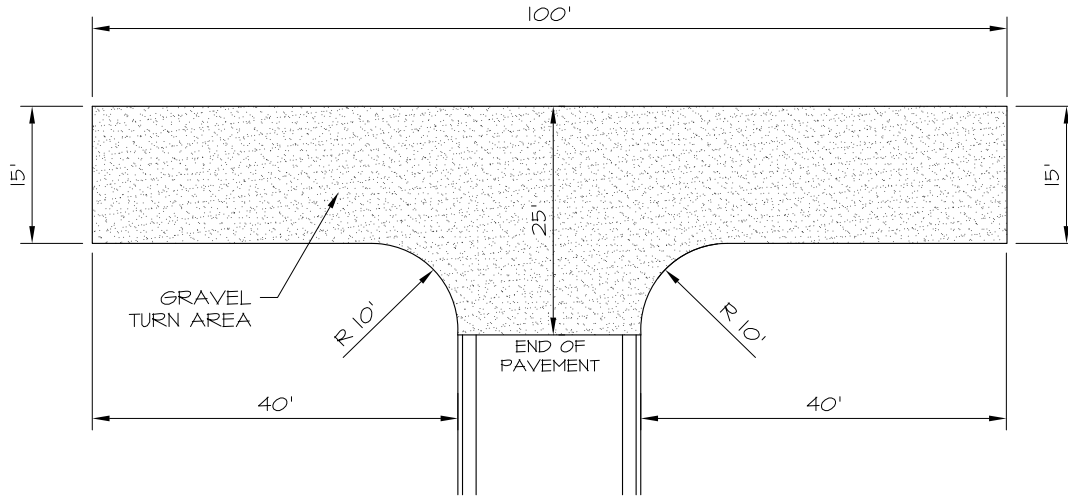
APPROACH GRADE
N.T.S.



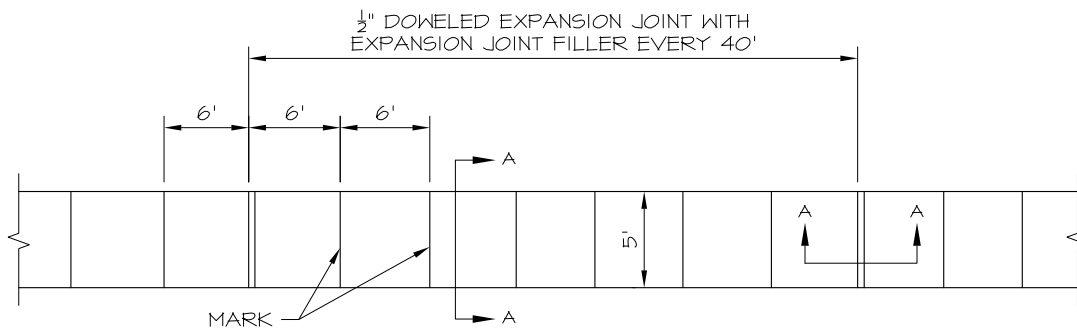
CONTRACTION JOINTS
N.T.S.

NOTES:

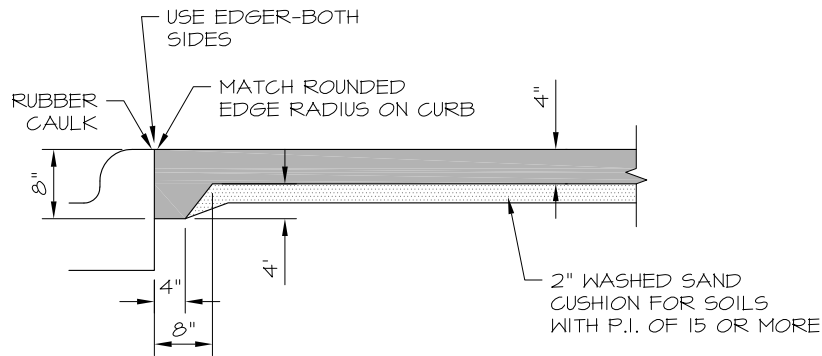
1. DRIVEWAY TO MATCH TYPICAL STREET SECTION TO ROW LINE.
2. 30' MIN. JOINTING. VARIATIONS TO BE APPROVED BY CITY ENGINEER.



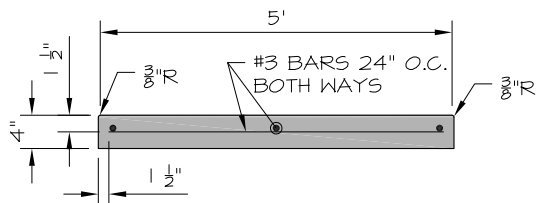
TEMPORARY T TURN AROUND
N.T.S.



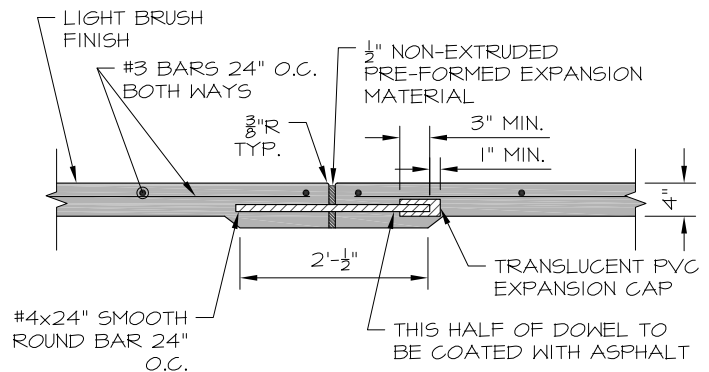
PLAN
N.T.S.



**JOINT LUG DETAIL FOR MEDIAN PAVEMENT
OR SIDEWALK ADJACENT TO CURB**
N.T.S.



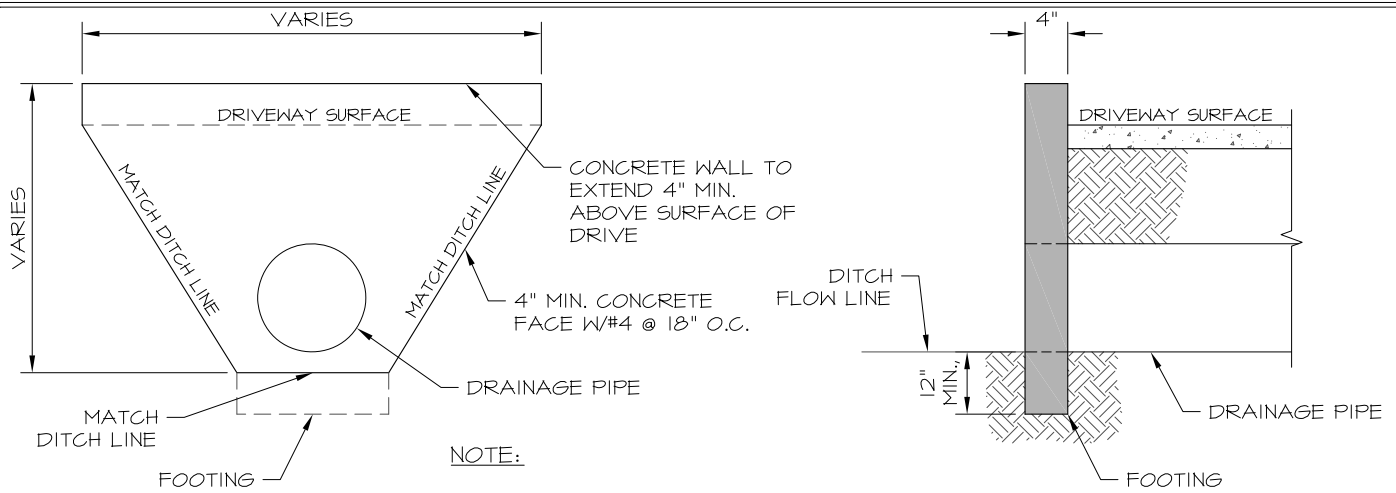
SECTION A-A
N.T.S.



SECTION B-B
N.T.S.

NOTES:

1. CROSS SLOPE OF SIDEWALK SHALL BE $\pm \frac{1}{4}$ " PER FT. MIN. TO $\pm \frac{3}{8}$ " PER FT. MAX.
2. SIDEWALK TO BE CLASS A CONCRETE (3,000 PSI).
3. ALL HONEYCOMB IN BACK OF CURB TO BE TROWEL-PLASTERED BEFORE POURING SIDEWALK.
4. LUG MAY BE FORMED BY SHAPING SUBGRADE TO APPROXIMATE DIMENSIONS SHOWN.

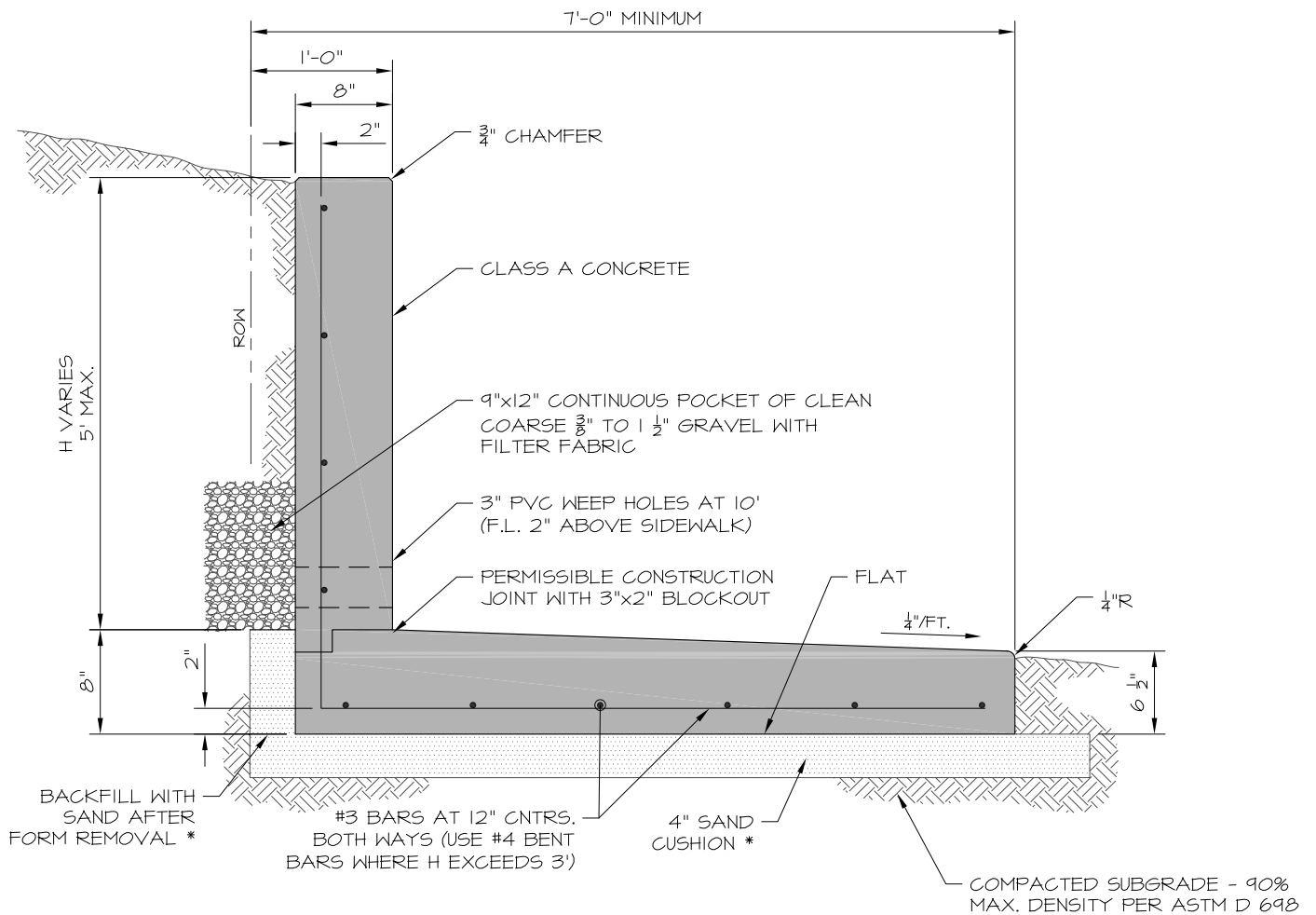


NOTE:

1. CONCRETE THICKNESS IS MINIMUM REQUIRED. FINAL DESIGN TO BE SUBMITTED AND STAMPED BY A PROFESSIONAL ENGINEER.

PRIVATE DRIVEWAY HEADWALL

N.T.S.



* WHEN SPECIFIED ON PLANS

RETAINING WALL WITH INTEGRAL SIDEWALK

N.T.S.

NOTES:

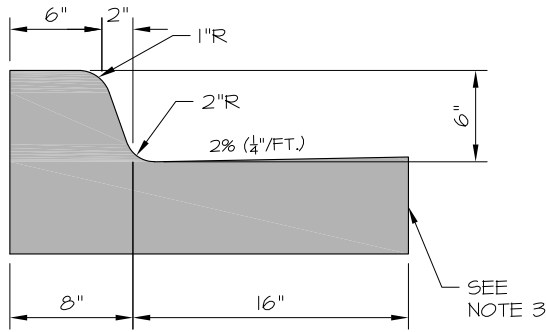
1. PROVIDE VERTICAL EXPANSION IN WALL AT 25' MAX. SPACING (USE EXPANSION JOINT, STANDARD DRAWING NO. 2050, AND MODIFY AS REQUIRED).
2. WALL DESIGN ASSUMES NO SURCHARGE. A SPECIAL ENGINEERING ANALYSIS IS REQUIRED FOR OTHER CONDITIONS.
3. WALL DESIGN IS MINIMUM STANDARDS. ACTUAL DESIGN TO BE STAMPED BY A PROFESSIONAL ENGINEER.

ST 15

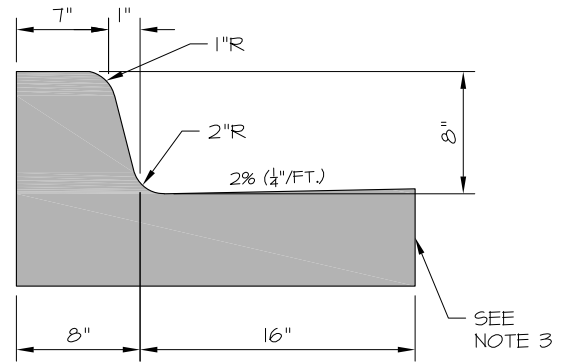
DESIGN STANDARD DETAILS

CITY OF CHOCTAW, OKLAHOMA

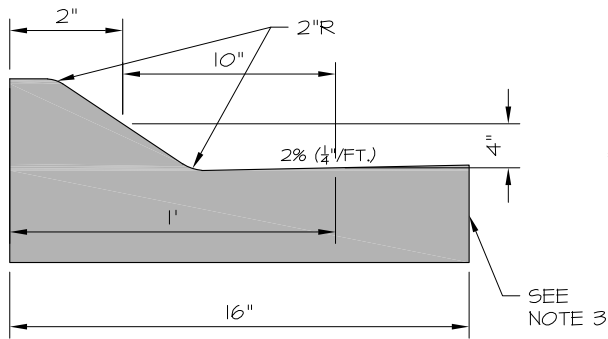
MISCELLANEOUS CONCRETE DETAILS



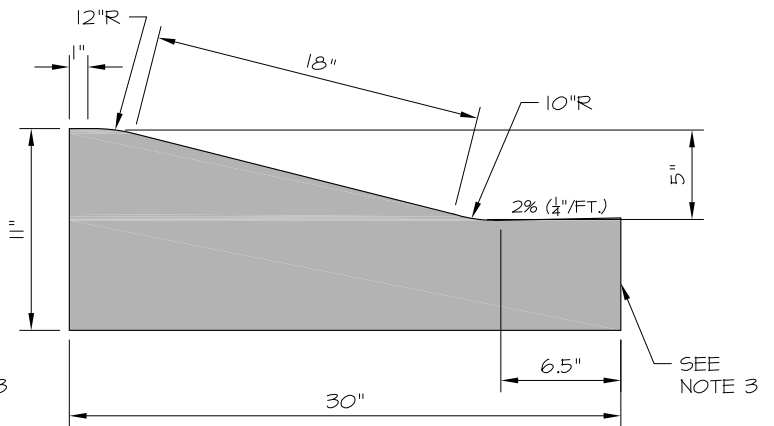
6" BARRIER CURB & GUTTER
N.T.S.



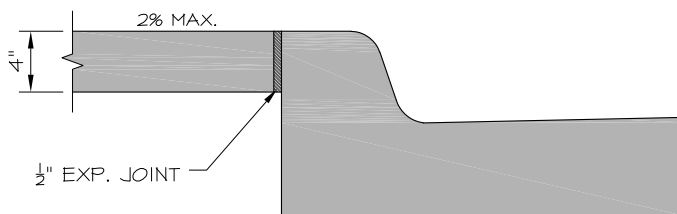
8" BARRIER CURB & GUTTER
N.T.S.



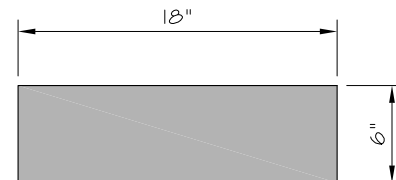
4" MOUNTABLE CURB & GUTTER
N.T.S.



5" MOUNTABLE CURB & GUTTER
N.T.S.



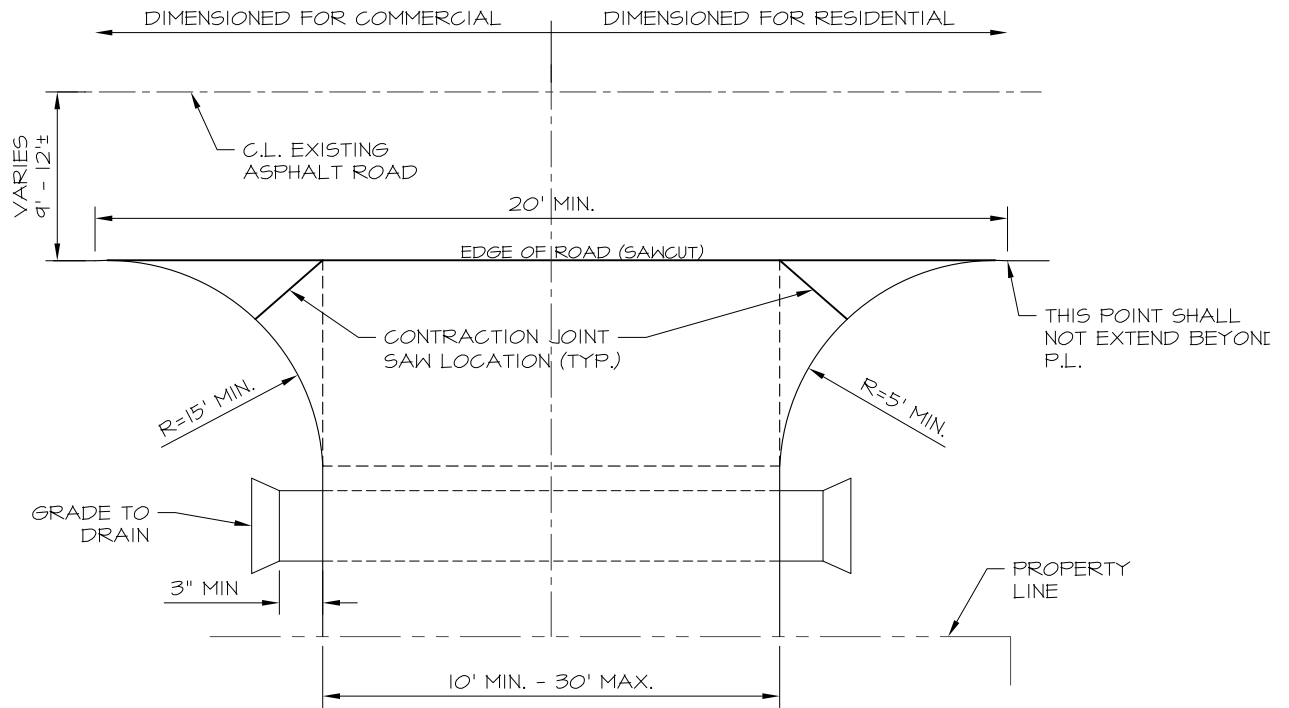
SIDEWALK ADJACENT TO CURB
N.T.S.



RIBBON CURB
N.T.S.

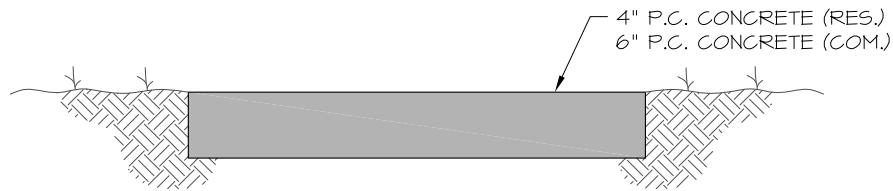
NOTES:

1. SEE ODOT STANDARD C5CD FOR JOINT DETAILS.
2. #4 TIE BARS 2'-6" LONG REQUIRED AT 18" CENTERS WITH TONGUE AND GROOVE JOINT IF CURB AND GUTTER NOT CAST INTEGRALLY WITH STREET PAVING. LONGITUDINAL CONSTRUCTION JOINTS ON LOCAL AND COLLECTOR STREET MAY, AT THE OPTION OF THE CITY, BE BUTT TYPE JOINTS WITH TIEBARS OR KEYWAY TYPE JOINT WITHOUT TIEBARS.
3. 6" MIN. WHEN CURB AND GUTTER IS POURED SEPARATELY IF CURB AND GUTTER IS POURED MONOLITHICLY WITH THE PAVEMENT THICKNESS. USE 1/2" DIA. DOWELS 18" LONG AT 24" CENTERS (SMOOTH OR DEFORMED) TO TIE CURB TO CONCRETE STREET PAVEMENT.
4. FOR ARTERIAL (URBAN) STREET SECTIONS, TIE GUTTER CROSS SLOPE SHALL BE 3%.



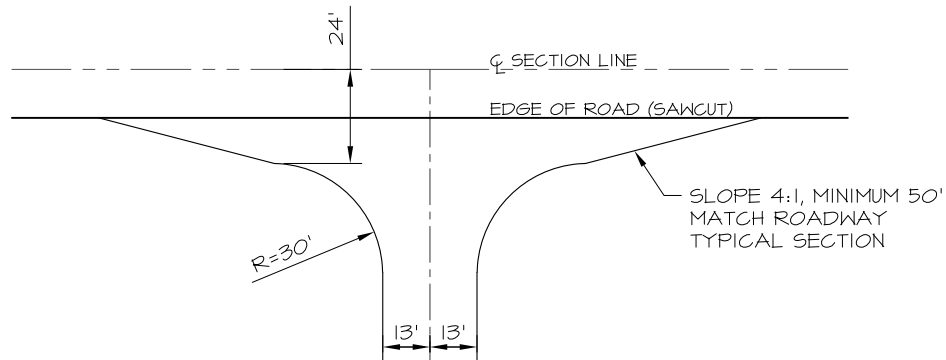
NOTES:

1. THIS DRIVEWAY DETAIL IS APPROVED FOR USE WHERE CURBS ARE NOT EXISTING ALONG THE STREET.
2. ALL DRIVEWAY MAINTENANCE TO BE BY THE PROPERTY OWNER.
3. THIS DETAIL DOES NOT APPLY TO INDUSTRIAL DRIVES.



TYPICAL SECTION
N.T.S.

COMMERCIAL AND RESIDENTIAL DRIVES-NO STREET CURBS
N.T.S.



SECTION LINE WIDENING
N.T.S.